BY:Matthew Edelman

<u>LEGEND</u> 1. THE CONTRACTOR SHALL CHECK ALL DIMENSIONS AND ELEVATIONS PRIOR TO CONSTRUCTION. ANY CONFLICTS CONCERNING THE CONSTRUCTION AROUND EXISTING TREE EXISTING OBSTRUCTIONS PER THESE PLANS SHALL BE RESOLVED BETWEEN THE ENTRANCE TIE-IN CONTRACTOR AND THE FIELD ENGINEER. ENTRANCE TIE-IN 2. THE CONTRACTOR AND OTHERS SHALL PERFORM ALL WORK IN A MANNER THAT WILL ENSURE THE LEAST PRACTICAL OBSTRUCTION TO TRAFFIC, PEDESTRIANS, EXISTING PINE TREE RESIDENTS, AND BE CONSISTENT WITH SAFETY. EXISTING SIGN 3. BASELINE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. DIMENSIONS AND STATIONING ARE APPROXIMATE AND NOT A RESULT OF FIELD SURVEY. EXISTING LIGHT 4. THE CONTRACTOR WILL NOTIFY PROPERTY OWNERS 72 HOURS PRIOR TO RECONSTRUCTION OF DRIVEWAY ENTRANCES. EXISTING CURB INLET THE STATE HIGHWAY ADMINISTRATION OR THE TOWN OF GARRETT PARK DOES NOT WARRANT THE CORRECTNESS OF THE TOPOGRAPHIC OR UTILITY DATA PRESENTED HEREIN EXISTING SEWER MANHOLE AND IS NOT RESPONSIBLE FOR ANY CONCLUSIONS DRAWN FROM THEM. EXISTING TRANSFORMER 6. SIDEWALK RECONSTRUCTION LIMITS SHALL COINCIDE WITH CRACK CONTROL JOINTS (JOINTER GROOVES) OR EXPANSION JOINTS IN EXISTING SIDEWALK. - BACK OF CURB — BACK OF CURB FOREST TREELINE NEW EXPANSION JOINTS SHALL BE PROVIDED AT THE INTERFACE OF EXISTING AND PROPOSED SIDEWALK. SEE STANDARD MD. NO. 655.01. FACE OF CURB - FACE OF CURB PROPERTY BOUNDARY 7. CONTRACTOR TO INSTALL DETECTABLE WARNING SUFACE ON SIDEWALK RAMP GUTTER PAN GUTTER PAN AS PER STANDARD MD. NO. 655.40. --- --- EXISTING BUILDING 8. FORM NEW SIDEWALK AND EXPANSION JOINT MATERIAL AROUND EX. UTILITY POLES, MANHOLE COVERS, VALVE COVERS, AND FIRE HYDRANTS AS NECESSARY. EXISTING UNDERGROUND VARIES VARIES 9. EXISTING INLETS SHALL NOT BE DAMAGED DURING CONSTRUCTION. DAMAGE TO THE INLETS WILL REQUIRE THE INLET TO BE REPLACED AT NO ADDITIONAL COST TO THE DOWN DOWN EXISTING GAS LINE 10. RECONSTRUCT PROPERTY WALKWAYS TO TIE INTO PROPOSED SIDEWALK GRADES EXISTING SANITARY SEWER AS DIRECTED BY THE ENGINEER. TYPICAL DRIVEWAY WITH BUFFER TYPICAL DRIVEWAY WITHOUT BUFFER 11. EXISTING CURB HEIGHTS PROVIDED ON PLAN SHALL BE VERIFIED BY THE SD — EXISTING STORM SEWER CONTRACTOR. RAMP AND DRIVEWAY TRANSITION LENGTHS SHALL BE VERIFIED IN THE FIELD ACCORDING TO SHA STANDARD MD. NO. 630.01, MD. NO. 630.02, — — - 388— — — EXISTING CONTOUR MD. NO. 655.11, MD. NO. 655.12, AND MD. NO. 655.13. 12. STABILIZE ALL DISTURBED AREAS NOT DRAINING TO A SEDIMENT CONTROL EXISTING FENCE DEVICE AT THE END OF EACH DAY. THE CONTRACTOR SHALL LIMIT WORK ACTIVITIES SUCH THAT ALL DISTURBED AREAS CAN BE STABILIZED TO FINAL EXISTING CURB GRADE AND PLACEMENT OF THE TOP SOIL, SEED AND MULCH, SOD OR OTHER STABILIZATION METHODS CAN BE PERFORMED AT THE END OF EXISTING EDGE OF PAVEMENT 13. MAINTAIN ALL VEHICLE ACCESS POINTS AT RESIDENTIAL DRIVEWAYS AND EXISTING SIDEWALK UNSIGNALIZED INTERSECTIONS. EXISTING SPOT ELEVATION 14. MAINTAIN IN GOOD CONDITION ALL EXISTING TRAFFIC SIGNS. IF, IN THE OPINION OF THE ENGINEER, ANY SIGNS ARE DAMAGED BY THE CONTRACTOR'S PROPOSED CONTOUR OPERATION, THEY ARE TO BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE. 15. THE CONTRACTOR SHALL ADJUST ALL EXISTING VISIBLE MANHOLES, VALVE PROPOSED COMBINATION BOXES, INLETS AND OTHER STRUCTURES, AS PER SPECIFICATION SECTION CURB AND GUTTER PROPOSED CONCRETE 16. DO NOT LEAVE A PAVEMENT DROP-OFF AT ANYTIME AFTER A WORK DAY IS 17. MAINTAIN THE LIMITS OF WORK WITHIN THE PROJECT IN A SAFE AND NEAT PROPOSED ASPHALT MANNER THROUGHOUT THE DURATION OF THE CONTRACT. PILES OF DEBRIS SUCH AS EXCAVATED SOIL AND HMA ARE EXPECTED TO BE REMOVED OR STABILIZED WITHIN THE SAME WORKING DAY. FAILURE TO COMPLY WITH THIS REQUIREMENT MAY RESULT IN ADMINISTRATIVE ACTION AS OUTLINED UNDER FAILURE TO MAINTAIN PROJECT OF THE SPECIFICATIONS. 18. A RIGHT-OF-ENTRY AGREEMENT MUST BE OBTAINED FROM SHA DISTRICT OFFICE PRIOR TO THE COMMENCEMENT OF ANY STAGING ACTIVITIES SUCH AS THE ESTABLISHMENT OF A FIELD OFFICE OR FOR THE STORAGE OF EQUIPMENT AND MATERIAL WITHIN SHA RIGHT-OF-WAY. APPROVAL WILL REQUIRE THE EXECUTION 5'-0" OR AS DIRECTED (TYP.) OF THE AGREEMENT AND A RETAINAGE TO BE WITHHELD FROM PROGRESS PAYMENTS UNTIL SUCH TIME THE AFFECTED AREA(S) HAVE BEEN RESTORED TO PRE-USE CONDITION. IF ADDITIONAL DISTURBANCE IS NECESSARY, THE CONTRACTOR **DEPRESSED** MAY BE REQUIRED TO AMEND ANY APPLICABLE PERMITS THAT MAY BE AFFECTED CONCRETE CURB CONCRETE CURB & GUTTER AND WILL BE SUBJECTED TO ANY AND ALL PERMIT CONDITIONS. FOR SIDEWALK RAMPS FOR SIDEWALK RAMPS SIDEWALK IF SPECIFIED
IN THE CONTRACT DOCUMENTS 19. REFER TO ES-01 FOR EROSION AND SEDIMENT CONTROL REQUIREMENTS. 20. SEDIMENT AND EROSION CONTROL REGULATIONS WILL BE STRICTLY ENFORCED TYPE 'D' 21. SIDEWALK RAMP CONSTRUCTION WILL BE PAID AS 5 INCH CONCRETE SIDEWALK. GENERAL NOTES 22. WHERE CURB, CURB AND GUTTER, OR DEPRESSED CURB IS PROPOSED, THE SLOPE GUTTER PAN '2" PER FOOT TOWARD FLOW LINE ON ALL ROADWAYS INCLUDING SUPERELEVATED SECTIONS. EXCEPT INTERCHANGE RANPS. 1. REFER TO MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION SPECIFICATIONS FOR MATERIALS, (1) SLOPE GUTTER PAN 12" PER FOOT TOWARD FLOW LINE ON ALL ROADWAYS INCLUDING SUPERELEVATED SECTIONS. EXCEPT INTERCHANGE RAMPS. PROPOSED CURB AND GUTTER CONTRACTOR SHALL REPLACE THE ADJACENT CURB OR CURB AND GUTTER TO METHODS OF CONSTRUCTION AND EXPANSION JOINT LOCATIONS. ② ROADWAY PAVEMENT SLOPE.
③ ROADWAY PAVEMENT CONSTRUCTION JOINT. THE EXISTING CURB JOINT. 2 ROADWAY PAVEMENT SLOPE. 2. THIS STANDARD SHALL BE USED ON PRIMARY, SECONDARY AND TERTIARY RESIDENTIAL ROADS EXCEPT PROVIDE LONGITUDINAL TIE DEVICE "J" BAR MODIFIED. REFER TO STANDARD NO ND 572.61.) ROADWAY PAVEMENT CONSTRUCTION JOINT. AT CURB RETURNS AND INLET THROATS. 2" ASPHALT PATCH -PROVIDE LONGITUDINAL TIE DEVICE "J" BAR MODIFIED. REFER TO STANDARD NO ND 572.61. 23. CONTRACTOR SHALL PROVIDE ENTRANCE TIE-IN AS NECESSARY TO PROVIDE ADEQUATE (5) FLOW LINE. TRANSITION FROM RECONSTRUCTED ENTRANCE TO EXISTING PAVEMENT (SEE MD 630.01 3. WHENEVER STANDARD MC-100.01 CURB IS USED IN CONJUNCTION WITH THIS STANDARD, A TEN FOOT TRANSITION SHALL BE PROVIDED FROM STANDARD MC-101.01 TO STANDARD MC-100.01 FOR CURB AND MD 630.02); PAVEMENT MATERIAL SHALL MATCH EXISTING. EXISTING ROADWAY RETURNS AND CURB SECTIONS WHICH INCLUDE INLETS. 24. HMA FOR DRIVEWAYS WILL BE PAID AS SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE. PAVEMENT A. RIGID PAYEMENT ROADWAY ADJACENT TO COMBINATION CURB AND GUTTER AND CLOSED SECTION ROADWAY USING RIGID PAYEMENT WITH COMBINATION CURB AND GUTTER SHALL BE KEYED AND TIED AT THE ROADWAY PAYEMENT CONSTRUCTION JOINT. REFER TO STANDARD MD 572-61 FOR METHOD OF KEYMAY AND LONGITUDINAL TIE DEVICES. SPACING OF THE TIE BARS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS. RIGID PAYEMENT AND CURB SHALL BE CONSTRUCTED AS INDICATED. THE DEVICES AND KEYS ARE NOT REQUIRED WHEN USING FLEXIBLE PAYEMENT FOR ROADWAY.

B. MAXINUM JOINT SPACING FOR CONCRETE CURB AND COMBINATION CURB & GUTTER IS 10°. SEE SPECIFICATION FOR LOCATIONS AND DESCRIPTION OF TREATMENT FOR THE TYPES OF JOINTS USED. A. RIGID PAVEMENT ROADWAY ADJACENT TO COMBINATION CURB AND GUTTER AND CLOSED SECTION ROADWAY USING RIGID PAVEMENT WITH COMBINATION CURB AND GUTTER SHALL BE TIED AT THE ROADWAY PAVEMENT CONSTRUCTION JOINT. REFER TO STANDARD MD 572-61 FOR METHOD OF LONGITUDINAL TIE DEVICES. SPACING OF THE TIE BARS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, RIGID PAVEMENT AND CURB SHALL BE CONSTRUCTED AS INDICATED. TIE DEVICES ARE NOT REQUIRED WHEN USING FLEXIBLE PAVEMENT FOR ROADWAY. THE STANDARD DISTANCE BETWEEN JOINTS SHALL BE TEN FEET (MAXIMUM AND MINIMUM DISTANCES SHALL BE THIRTEEN FEET AND FIVE FEET RESPECTIVELY). PG 64S-22, LEVEL 4 (2 INCH DEPTH). 5. EXPANSION JOINT MATERIAL SHALL BE 1/2 INCH PREFORMED CORK, TRIMMED AND SEALED WITH NON-STAINING TWO-COMPONENT POLYSULFIDE OR POLYURETHANE ELASTOMERIC TYPE SEALANT 25. ADJUST EXISTING UTILITIES IMPACTED BY THE SIDEWALK & SIDEWALK RAMP B. MAXIMUM JOINT SPACING FOR CONCRETE CURB AND COMBINATION CURB & GUTTER IS 10'. SEE SPECIFICATION FOR LOCATIONS AND DESCRIPTION OF TREATMENT FOR THE TYPES OF JOINTS USED. COMPLYING WITH ASTM-C920. THE COST IS INCIDENTAL TO SIDEWALK AND SIDEWALK RAMP CONSTRUCTION. TYPE C OR D COMBINATION CURB AND GUTTER SHALL BE USED FOR ALL APPLICABLE NEW CONSTRUCTION AND IN THOSE AREAS WHERE THE COMBINATION CURB AND CUTTER IS TO BE REPLACED IN KIND. D. TYPE C OR D CURB SHALL BE USED FOR THE REPLACEMENT OF LIKE KIND OF CURB ONLY. NOT TO BE USED FOR NEW CONSTRUCTION EXCEPT WHERE INDICATED ON APPROPRIATE INLET STANDARDS. 26. THE CONTRACTOR SHALL RESET ANY SIGNS DISTRUBED BY THE CONSTRUCTION AS DIRECTED BY THE ENGINEER. APPROVED 14 APR 106 REVISED MONTGOMERY COUNTY SPECIFICATION CATEGORY CODE ITEMS SPECIFICATION CATEGORY CODE ITEMS DEPARTMENT OF PUBLIC WORKS & TRANSPORTATION Maryland Department of Transportation **Maryland Department of Transportation** STATE HIGHWAY ADMINISTRATION STATE HIGHWAY ADMINISTRATION COMBINATION CONCRETE STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES DIRECTOR, DEPT. OF PUBLIC WORKS & TRANSPORTATION MAINTENENCE OF TRAFFIC NOTES: CURB AND GUTTER STANDARD TYPES C AND D DEPRESSED CURB FOR COMBINATION TYPE II CONCRETE · TYPE C CURB AND GUTTER AND DEPRESSED CONCRETE CURB AND COMBINATION Holger Seriamo 1. MD SHA STANDARD PLATES ARE TO BE USED FOR MAINTENENCE OF TRAFFIC. CURB FOR SIDEWALK RAMPS CONCRETE CURB & GUTTER STANDARD NO. MC-101.01 COMPACTED STONE BASE STANDARD NO. STANDARD NO. MD 620.03 MD 620.02-01 CHIEF, DIV. OF CAP. DEV. 2. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE MOT STANDARDS MD 104.00-01 THRU 104.06-25. TYPICAL TYPE 'D' COMBINATION CURB AND GUTTER DETAIL TYPICAL DEPRESSED CURB DETAIL TYPICAL ROLLED CURB DETAIL 3. NO WORK IS TO BEGIN UNTIL ALL ADVANCE WARNING SIGNS, DRUMS AND ARROW TYPICAL PAVEMENT PATCHING DETAIL PANELS ARE IN PLACE AND OPERATIONAL. 4. ADVANCED NOTIFICATION OF SIDEWALK CLOSURES SHALL BE PROVIDED. A BARRIER THAT IS DECTECTABLE BY A PERSON WITH A VISUAL DISABILITY SHALL BE PLACED ACROSS THE FULL WIDTH OF THE CLOSED SIDEWALK. 5. WHERE PEDESTRIANS ARE DIRECTED TO CROSS TO THE OPPOSIDE SIDE OF THE ROADWAY. APPROPRIATE ADVANCED WARNING SIGNS SHOULD BE PLACED AT THE INTERSECTIONS RATHER THAN MID-BLOCK CROSSINGS. 6. TRAFFIC CONTROL DEVICES AND OTHER CONSTRUCTION MATERIALS AND FEATURES SHALL NOT INTRUDE INTO THE USABLE WIDTH OF THE SIDEWALK, TEMPORARY OR TOWN OF GARRETT PARK DATUM: NAD 83/91 Horizontal PEDESTRIAN FACILITY DESIGN SERVICES 65% DESIGN DRAWINGS NAVD 88 Vertical SAFE ROUTES TO SCHOOL (SRTS) GENERAL NOTES AND TYPICAL DETAILS CROSS REFERENCE REVISIONS ITEM SHEET NOs. SCALE N.T.S. ADVERTISED DATE OCT. 2017 CONTRACT NO. PROFESSIONAL CERTIFICATION: I CERTIFY THAT THESE DOCUMENTS GENERAL NOTES AND DETAILS WERE PREPARED OR APPROVED BY TYPICAL SECTIONS ME, AND THAT I AM A DULY LICENSED COUNTY __ MONTGOMERY DESIGNED BY ____ Engineering a Sustainable Future PROFESSIONAL ENGINEER UNDER THE GEOMETRIC LAYOUT 4 – 5 Nobis Engineering, Inc. LAWS OF THE STATE OF MARYLAND. DRAWN BY ____ LOGMILE SIDEWALK PLAN 6 - 10 20410 Century Boulevard, Suite 230 LICENSE NO. <u>31168</u> EROSION & SEDIMENT CONTROL PLAN 11 - 16 CHECKED BY _____ HORIZONTAL SCALE _ Germantown, MD 20874 EROSION & SEDIMENT CONTROL -17 – 18 EXPIRATION DATE: 1/12/2019 T(301) 528-2010 DETAILS AND NOTES F.A.P. NO. SEE TITLE SHEET VERTICAL SCALE STREAM RESTORATION PLAN AND DETAILS www.nobiseng.com Client - Focused, Employee - Owned DRAWING NO. GN-01 **OF** SHEET NO. X OF X

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